

Bike to Work Breakfast WA 2000-2004

Dr Alexandra McManus

and

Ms Jenny Smith

WA Centre for Health Promotion Research
Curtin University of Technology



The problem

- Perth - highest prop. cars per capita
- ~ 240,000 vehicle trips < 1km
- 6% of these trips – walking or cycling
- Perth Metropolitan Transport Strategy
 - increase # short trips
 - to 8% by 2010
 - to 11.5% by 2029



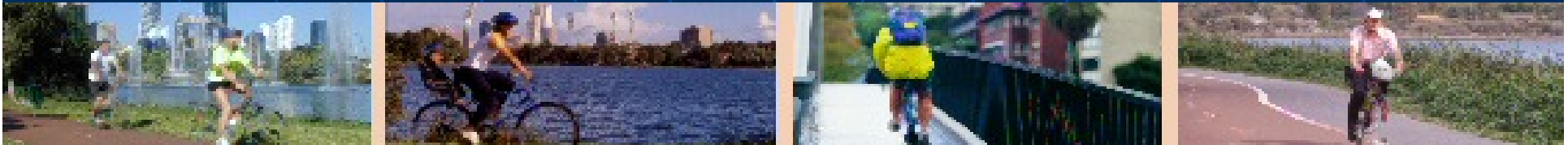
Cycling as a viable transport option

- Opportunity to promote physical activity
- Benefits:
 - health
 - environmental
 - economic



Cycling as a viable transport option

- In addition to the health benefits, increasing active transport (eg. cycling) within the community will:
 - reduce private vehicle use
 - reduce road congestion
 - reduce environmental pollution
 - reduce the demands made on public transport systems and inner city parking
 - reduce current trends of physical inactivity in our community



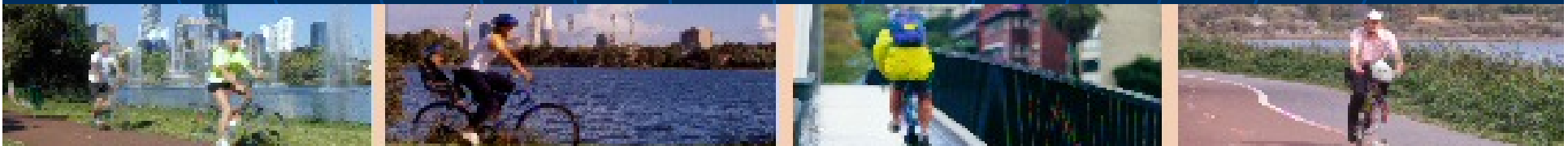
Workplace benefits

- **Cycling to work**
 - more cost-effective than most workplace healthy lifestyle programs
 - greater CV benefits
 - Improves morale and productivity
- **Physical activity as daily routine**
 - most effective strategy



Key determinants as a viable form of transport

- supportive urban planning
- supportive intersectoral approach
- supportive public policy
- provision of facilities
- TG - chars, attitudes, knowledge, skills, physical environment
- accepted as normal and safe part of life



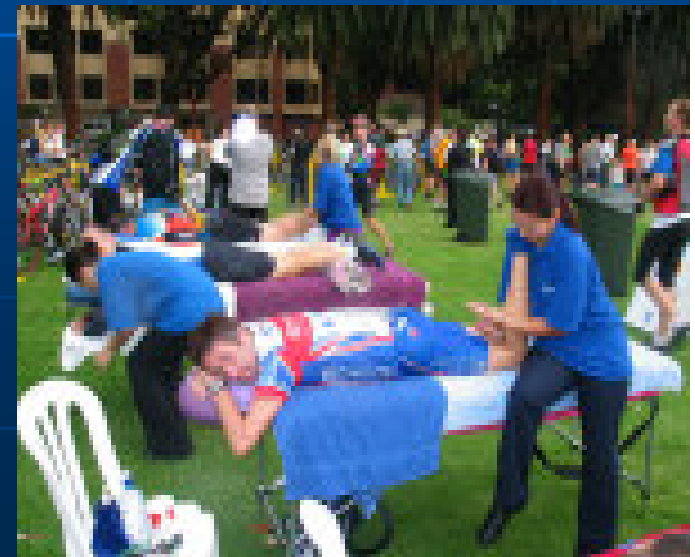
The Bike to Work Breakfast

- An initiative of the DPI since 1988
- Annual event held during Bike Week
- If cycle to work - healthy breakfast provided
- Raise awareness of the benefits of cycling
- Encourage regularly cycle to and from workplace

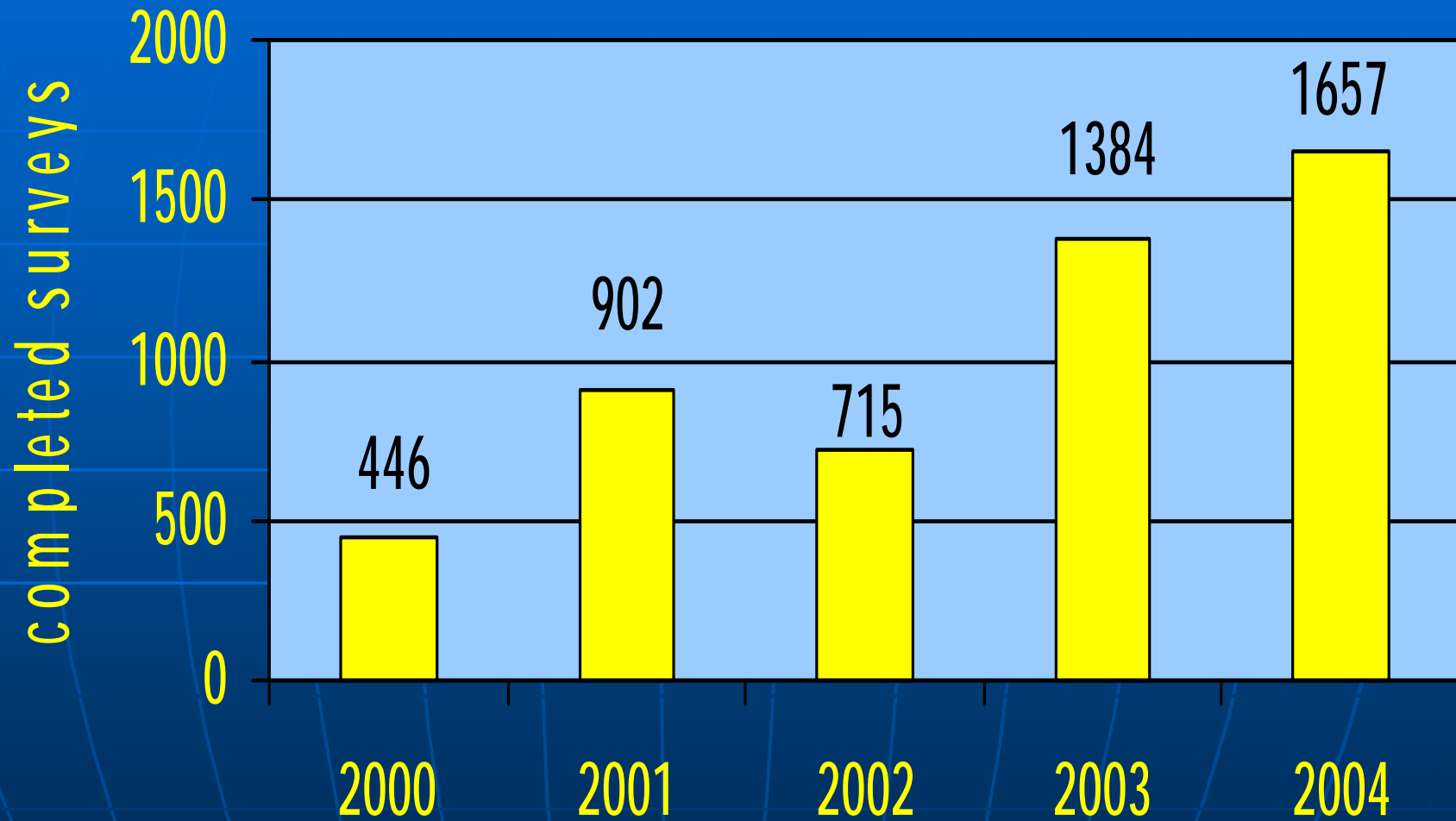


Bike to Work Breakfast 2000-2004

- Evaluated by WACHPR since 2000
- Trends from 2000 to 2004 compared
- SAQ – convenient sample at breakfast
- 2000 n = 446
- 2004 n = 1657



Bike to Work Breakfast 2000-2004



Reasons for participation 2000-2004

- Fitness (>80%) and enjoyment (>70%)
- Other reasons included:
 - cost benefits compared to driving
 - alternative form of transport
 - reduce stress and tension
- Environmental concerns decreased

Reason for cycling 2000-2004

Reason for cycling	Year				
	2000 (%)	2001 (%)	2002 (%)	2003 (%)	2004 (%)
Fitness	392 (90)	789 (87)	614 (86)	1165 (84)	1460 (88)
Enjoyment	330 (74)	650 (72)	515 (72)	892 (65)	1172 (71)
Environment	241(54)	481 (53)	302 (42)	458 (33)	649 (39)
Cost Effective	211(47)	427 (47)	313 (44)	521 (38)	742 (45)
Means of transport	N/A	N/A	337 (47)	646 (47)	759 (46)
Reduce stress & tension	198 (44)	405 (45)	334 (47)	425 (31)	737 (44)
Total participants (%)	446 (100)	902 (100)	715 (100)	1384 (100)	1657 (100)

Cycling-related trends and behaviours 2000-2004

- Trends in freq and distance relatively stable
- > 75% of respondents would wear helmets even if not compulsory

Recommendations for future strategies

- Facilities and initiatives based on needs of WA cycling population
- Strategies should reflect
 - characteristics of people who cycle regularly
 - factors that motivate them to cycle
 - diffs in cycling behaviour by age and gender

Recommendations for future evaluations

- Involve evaluators in development of instrument
- Follow-up to provide greater understanding of the barriers and enablers for cycling

